



Lewisville Downtown Plan Progress Report

Town of Lewisville Planning Board

Introduction

Nestled between the Yadkin River and the Muddy Creek, the Town of Lewisville is located in southwestern Forsyth County. This bedroom community of Winston-Salem was founded in the mid 1700's by Lewis Laugenour for whom the area is named. Incorporated as a Town is 1991, it is a growing upper middle class community encompassing approximately 12 square miles and is home to over 13,000 residents. In June of 2012, the Lewisville Planning Board and staff began work to create a new streetscape design for the proposed Great Wagon Road (GWR) and update the vision for the downtown area.

Limited access to public sewer and stormwater restrictions have attributed to the area's slow growth. However, it is recognized that as the commercial heart of the Town, growth is inevitable and must be addressed now to guide future investments.



Directives and Goals

Make changes to the LD1-S, LD2-S and Downtown Overlay Districts to include references to architectural design, scale, uses and streetscaping for the GWR Super Block.

Create a 'Super Block' ordinance for the properties located between David McKee Street and Tom Voss Road.

Change sideyard setbacks within the downtown to zero (0') feet and consider the creation of a new downtown zoning district LD3-S.

Create layout and design images for the Great Wagon Road (GWR) streetscape.

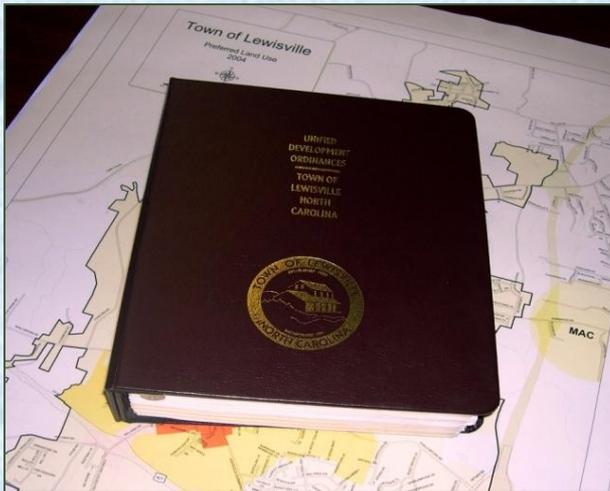


Revised LD1-S, LD2-S and Downtown Overlay Districts

As part of their work to revisit the current Downtown Plan, the Planning Board undertook the rewriting of the LD1-S, LD2-S and Downtown Overlay Ordinances.

Status:

- ✓ **Rewrote Purpose Statement**
- ✓ **Differentiated between the 'Core' Area and 'Gateway' Area and created separate standards for each**
- ✓ **Revised setbacks for the Core Area to encourage a more traditional Downtown look**
- ✓ **Rewrote Architectural Standards making them less subjective and more clearly defined**
- ✓ **Reduced the number of uses allowed within the Core and Gateway Areas**

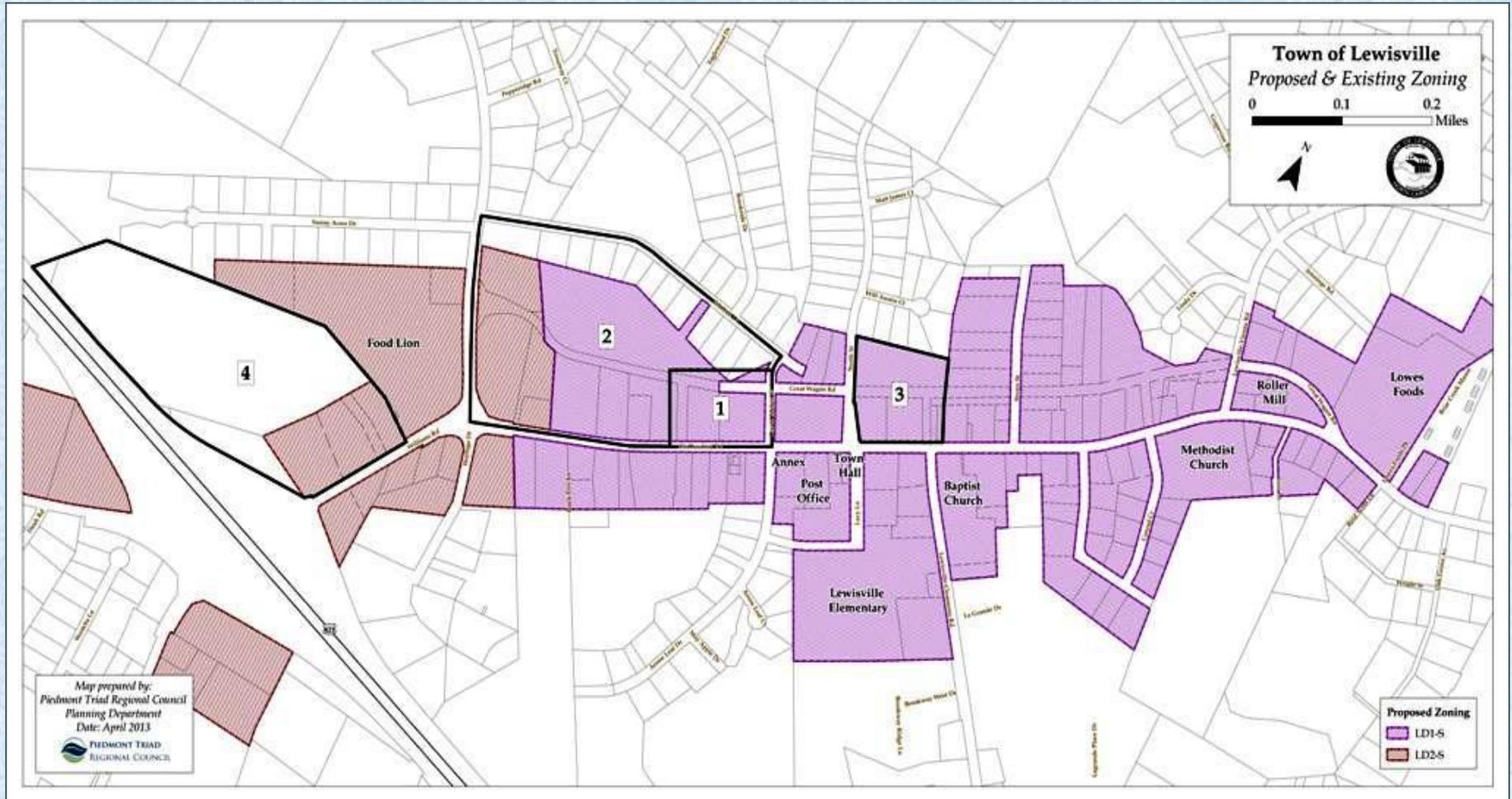


Revised LD1-S, LD2-S and Downtown Overlay Districts

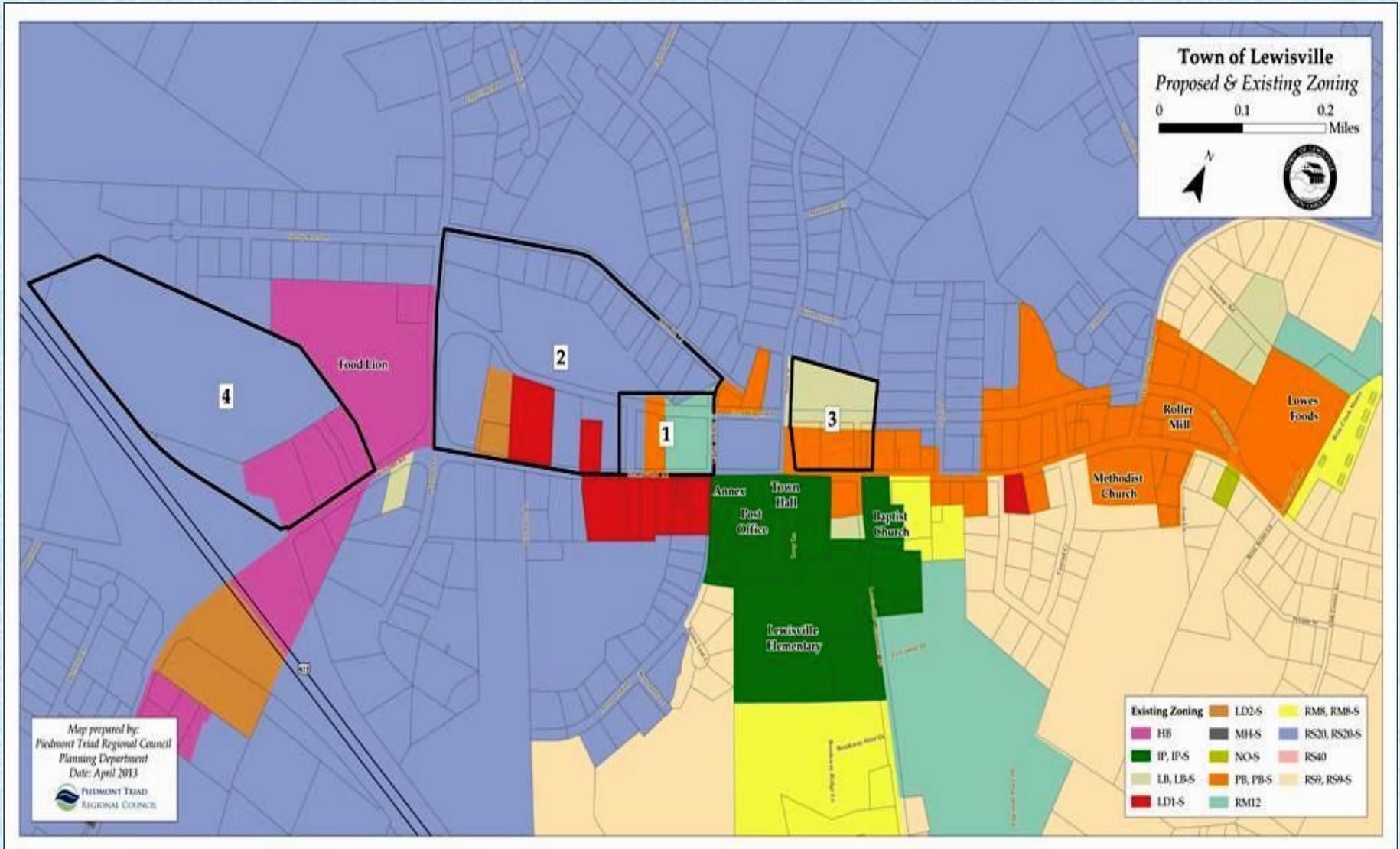
**Permitted
Uses
Reduced
within the
DCA and
GDA – Draft
Not Finalized**

Uses	Downtown Core Area (LD1-S)	Gateway District Area (LD2-S)	'Superblock' Area 1
Residential	Single-Family Homes Multi-family Homes		
Lodging	Bed & Breakfast Inns	Bed & Breakfast Inns Hotels/Motels	
Retail	Miscellaneous Retail Shops Food & Drug Pharmacy Combined Use General Merchandise Restaurant (without Drive-Thru) Banking and Finance Indoor Recreation Child Day Care Center	Miscellaneous Retail Shopping Centers Convenience Stores Gas Stations Combined Use Restaurant (with & without Drive-Thru) Banking and Finance	Miscellaneous Retail Shops Combined Use Restaurant (without Drive-Thru) Banking and Finance
Office/Retail	Professional Office Health Services Personal Services Medical & Dental Offices Business Services Nursing Care Center		Professional Office
Civic/Assembly	Community Center Government Offices Gallery or Museum Neighborhood Church Club or Lodge Library Public School Theater	Community Church Neighborhood Church Theater	
Other	Accessory Uses Transit Facilities Utilities	"Exploring Non- Conventional Uses Consistent with Lewisville Vision" Accessory Uses Transit Facilities Utilities	

Preferred LD1-S and LD2-S Areas

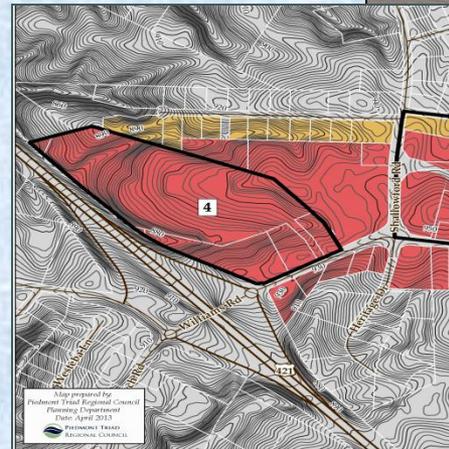


Current Downtown Zoning



Planning Area Overview

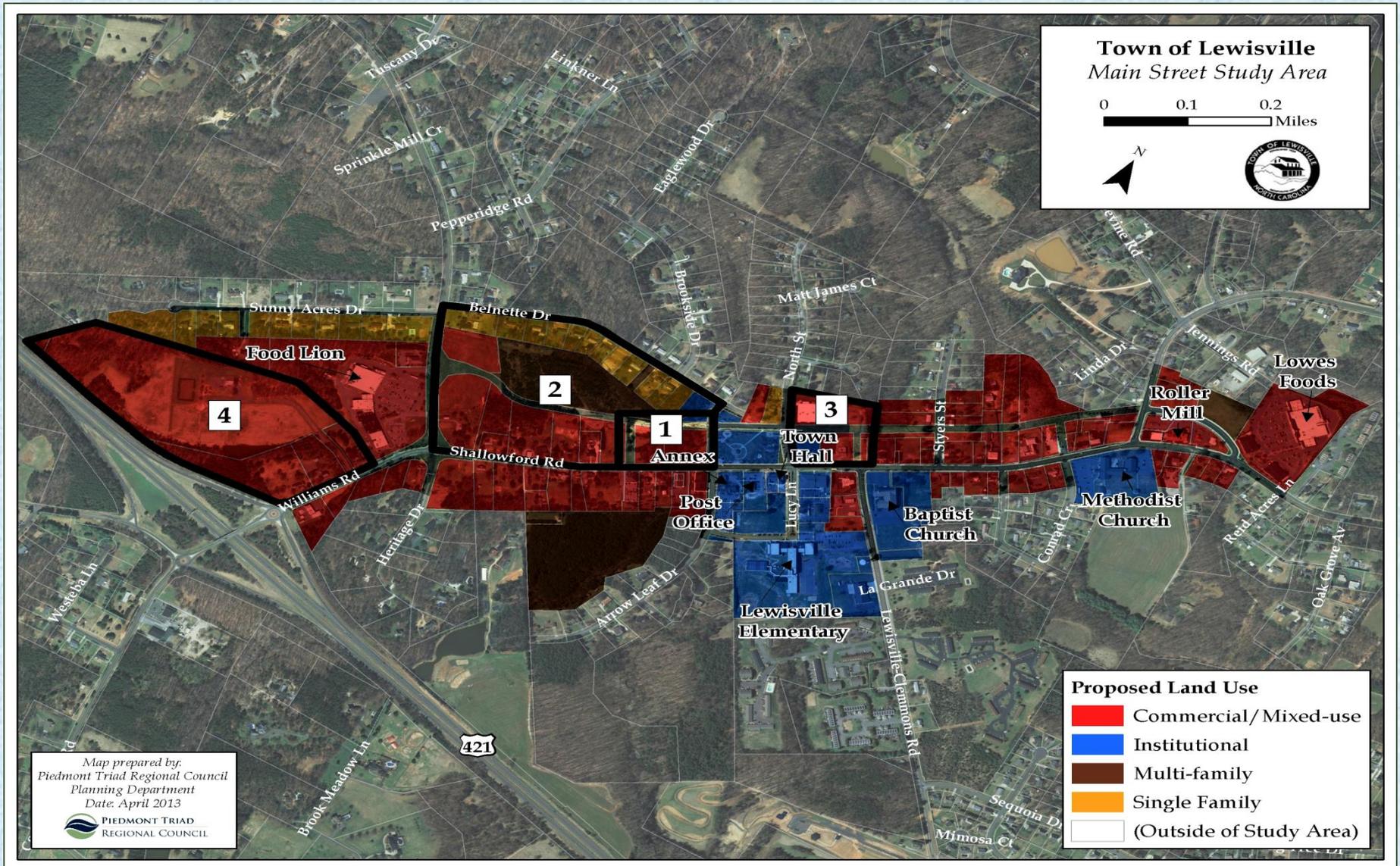
- ❑ The planning area consist primarily of smaller individual lots grouped between Shallowford Road and the proposed alignment of the Great Wagon Road. Other lands are located just north of the Great Wagon Road (GWR) bordering the Belnett Drive neighborhood. The entire area comprises approximately 120+/- acres within the downtown core commercial district.
- ❑ There are a number of existing business, institutional and residential uses located within the study area. The businesses are spread along Shallowford Road or found within the two existing shopping centers located at each end of the down-town district. Single-family residences are located along Shallowford Road and in compact neighborhoods adjacent to the planning area.
- ❑ Finally, it is important to highlight the importance of those parcels located adjacent to the US421 interchange at Williams Road and just behind the Oaks Shopping Center. Although just outside of the downtown core area, this area will eventually be attractive to developers because of its high visibility from US421. The potential exists for new large scale retailers, restaurants and micro-motels on this site.



Planning Area Overview



Planning Area Overview – Proposed Land Use and Focus Areas



Existing Conditions

As part of the Planning Board's initial review of the downtown, previous plans were obtained and critiqued to determine what had worked and what needed to be revisited. The following attributes were viewed as 'positive' by both members of the Board and residents.

**Shallowford Square Park –
Open Public Common**

**Buildings built to the 'Build To
Line'**

'Village Like' Residential Scale

Harmony of Architecture

**Pedestrian Friendly 'Very
Walkable'**



Existing Conditions

Small Town Atmosphere

Preservation of the Lewisville Roller Mill and Shops

Shallowford Road Streetscape Design

Clustering of Public and Governmental Buildings





AREA 1 – THE 'SUPERBLOCK'

The 'Superblock' — Aerial View

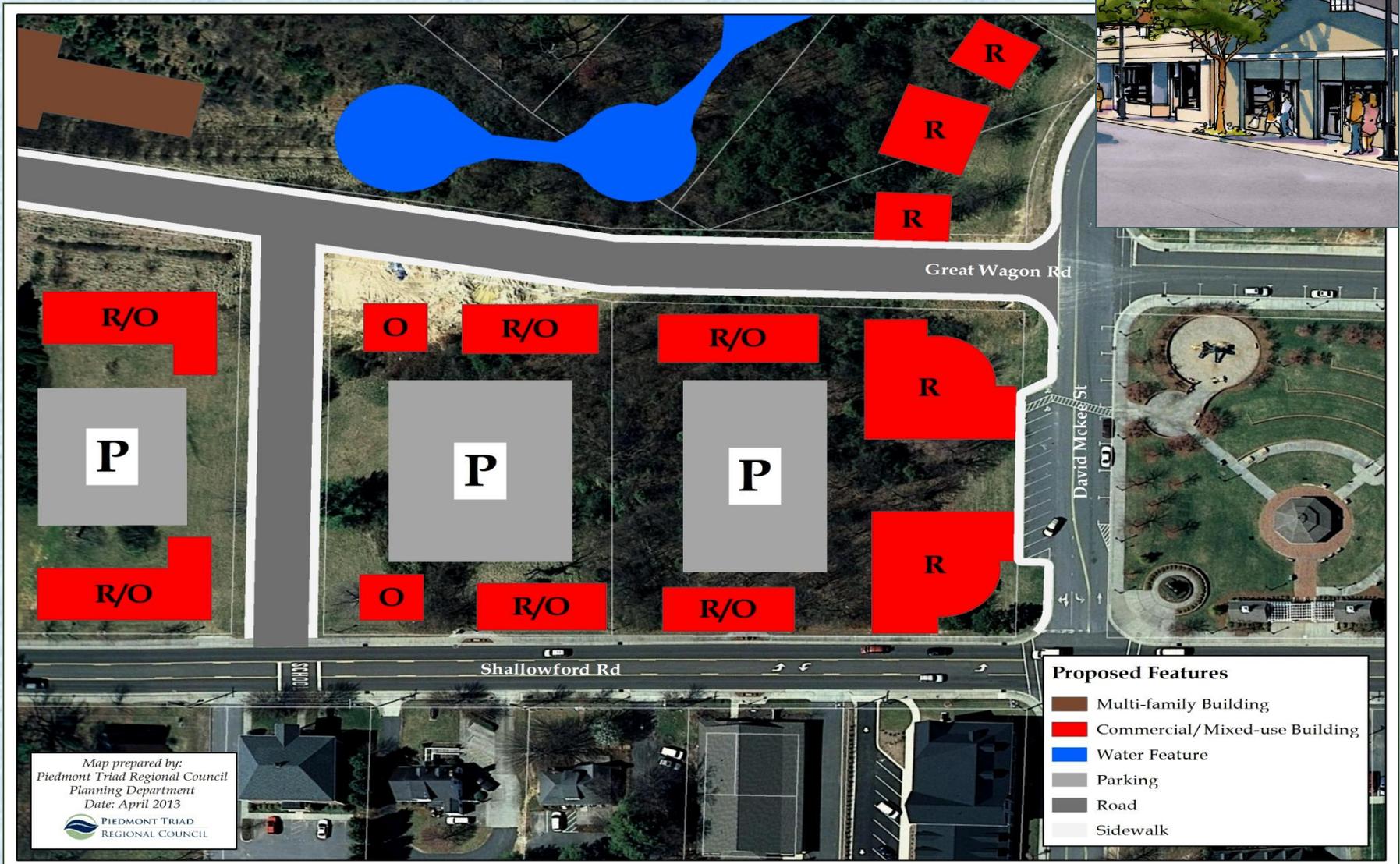


Retail/Office Uses – Area 1

Restaurants, coffee shops, small boutiques and shops, such as art galleries, apparel, garden & floral, etc. could be clustered within and around the 'Super Block' as it has been described by prior directives. This block should form the 'central core' or 'town center' that integrates specialty retail, housing, offices, entertainment for the downtown area. Recognized as a lifestyle center in the development industry, this development type offers a safe, convenient neighborhood shopping experience while also providing a sense of place to visitors and residents.



Design Plan - The 'Superblock' Area 1





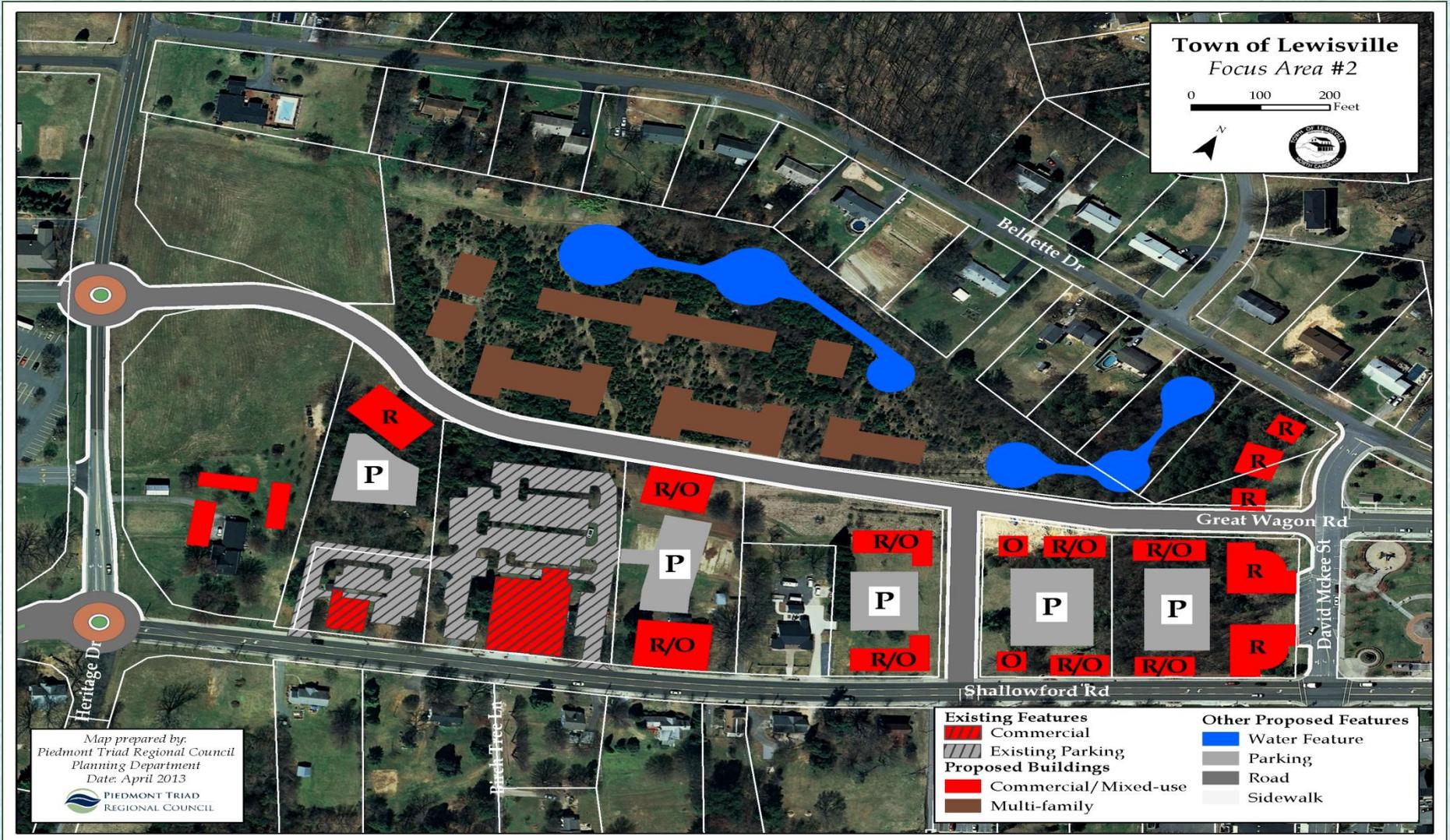
AREA 2 – RETAIL, MIXED USES AND MULTIFAMILY DWELLINGS

Residential Uses – Area 2

A mix of housing sizes, types and pricing could provide affordable, yet high quality housing, particularly for 1st time home-buyers and empty nester/retirees seeking one floor living. Housing from 1,200 to 2500 sq.ft. on one or two floors in a more dense pattern (townhome or small lots) would meet the needs of both markets. Rental housing should also be considered within the district and just beyond. The Kiger property could be / should be retained and re-purposed with modifications and sensitive additions to create a flagship gateway restaurant and/or bed and breakfast inn.



Design Plan - Area 2





AREA 3 – CULTURAL CENTER

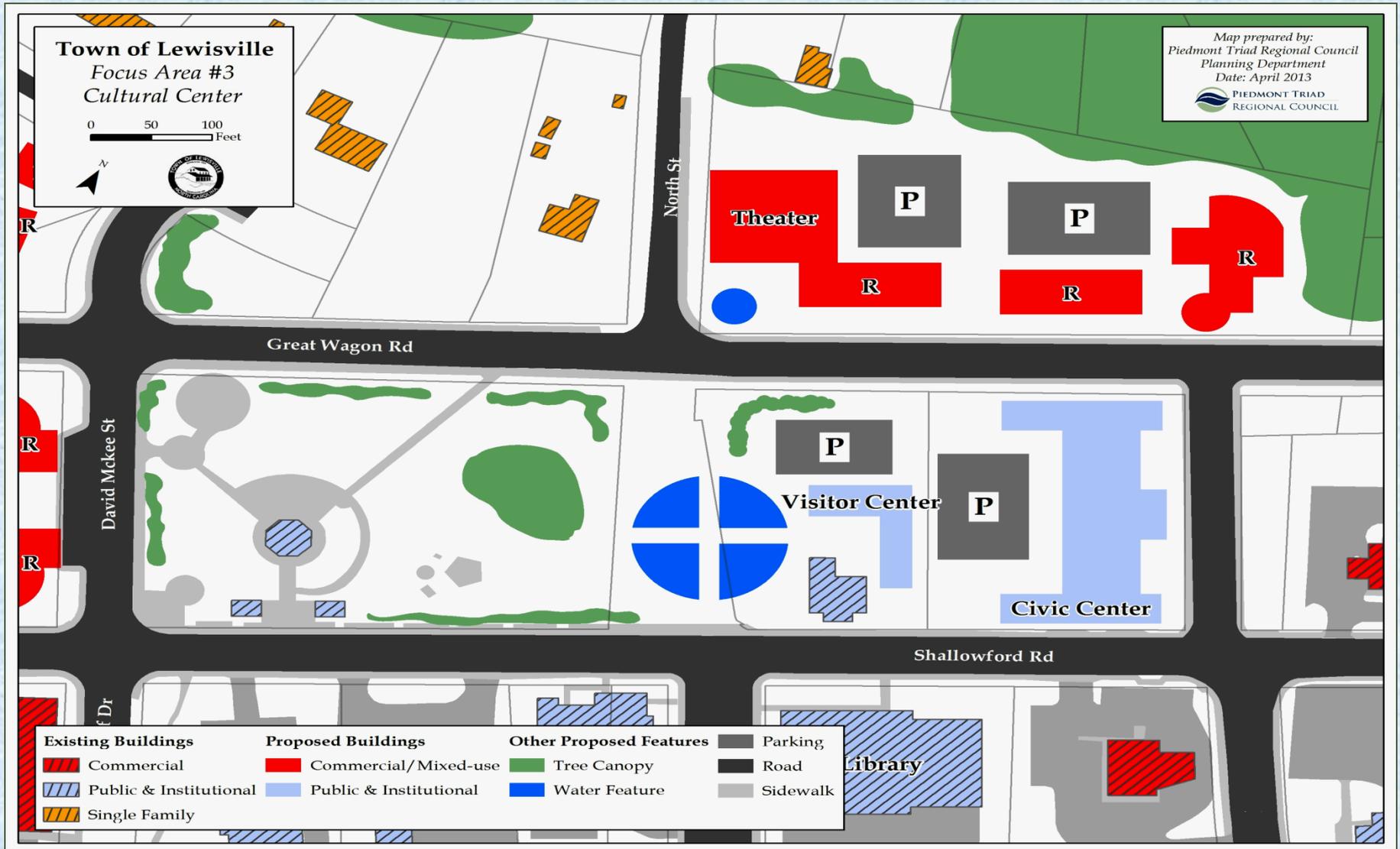
Cultural Center – Area 3

Entertainment and Community Recreation

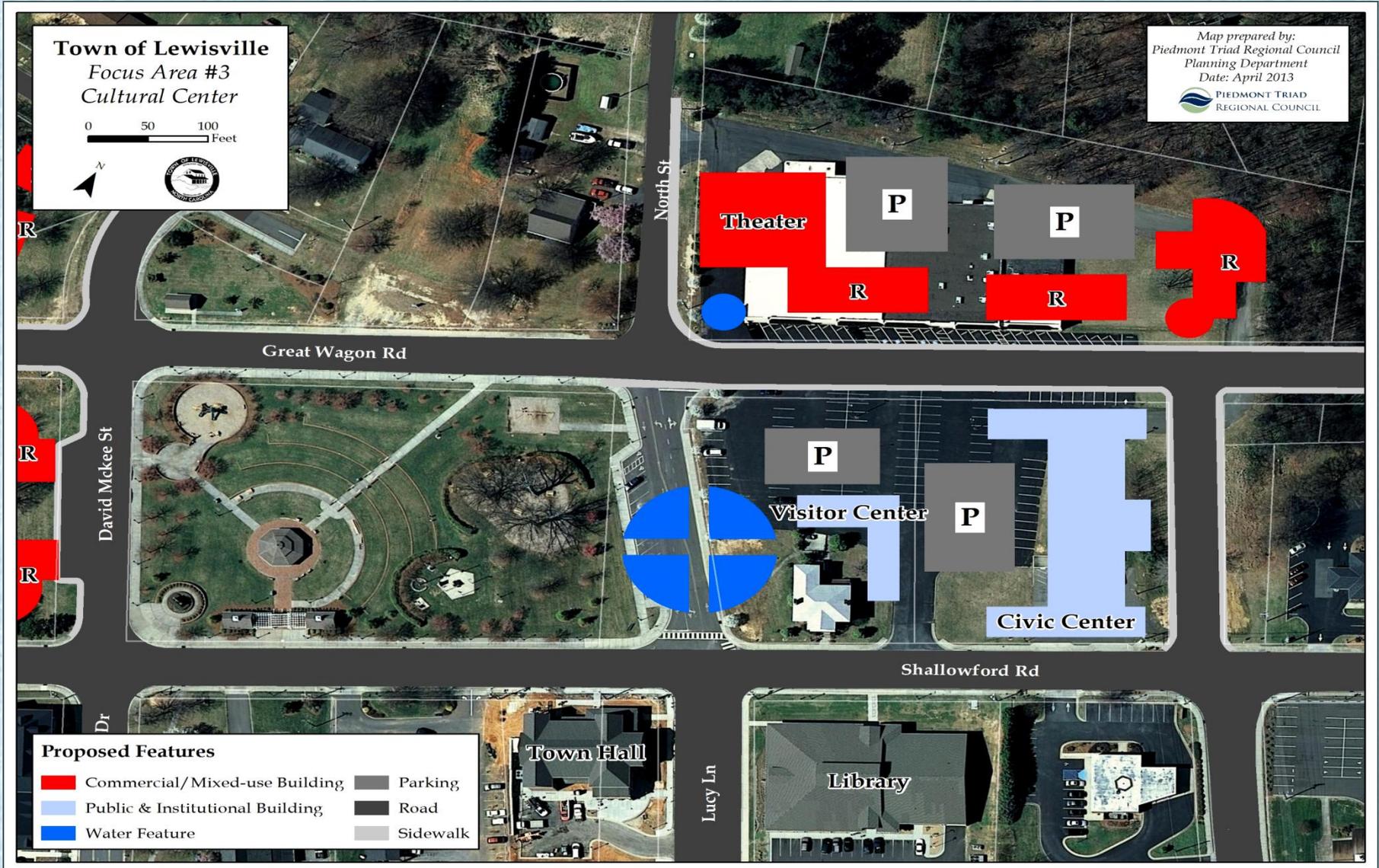
Intended improvements at the eastern end of the planning area could combine with existing amenities such as Shallowford Square Park. Previous suggestions include the closing of North Street to vehicular traffic and creating a pedestrian walkway that would tie the Laugenour home place to the park by the use of a water fountain or other feature. It has been also noted that there is only one theater located in western Forsyth that serves the Lewisville market. Given the district's proximity to the interstate and the opportunity for other complimentary uses such as restaurants, shops, etc. the viability of a theater operation seems viable. The design plan establishes a possible location for a new Community/Civic Center that would compliment the proposed Laugenour Historical Museum site.



Design Plan – Cultural Center Area 3



Design Plan – Cultural Center Area 3





AREA 4 – FUTURE GATEWAY DEVELOPMENT

Future Gateway Development – Area 4

The gateway area should encourage the creativity of the developer while ensuring appropriate high-quality design and site planning.

It is suggested that the design of Area 4 should emulate the look and feel of the downtown core while recognizing that the impact of vehicular traffic will play a large role.

The gateway area represents an opportunity to cement and reinforce the vision and values of Lewisville: rural agrarian heritage, small town charm and hometown livability. As a primary entrance into downtown, it has the potential to develop in both conventional and unconventional ways. The Planning Board is currently exploring best practices elsewhere in order to recommend final uses appropriately.



Design Plan — Future Gateway Development Area 4

Town of Lewisville
Focus Area #4
Future Commercial

0 100 200
Feet



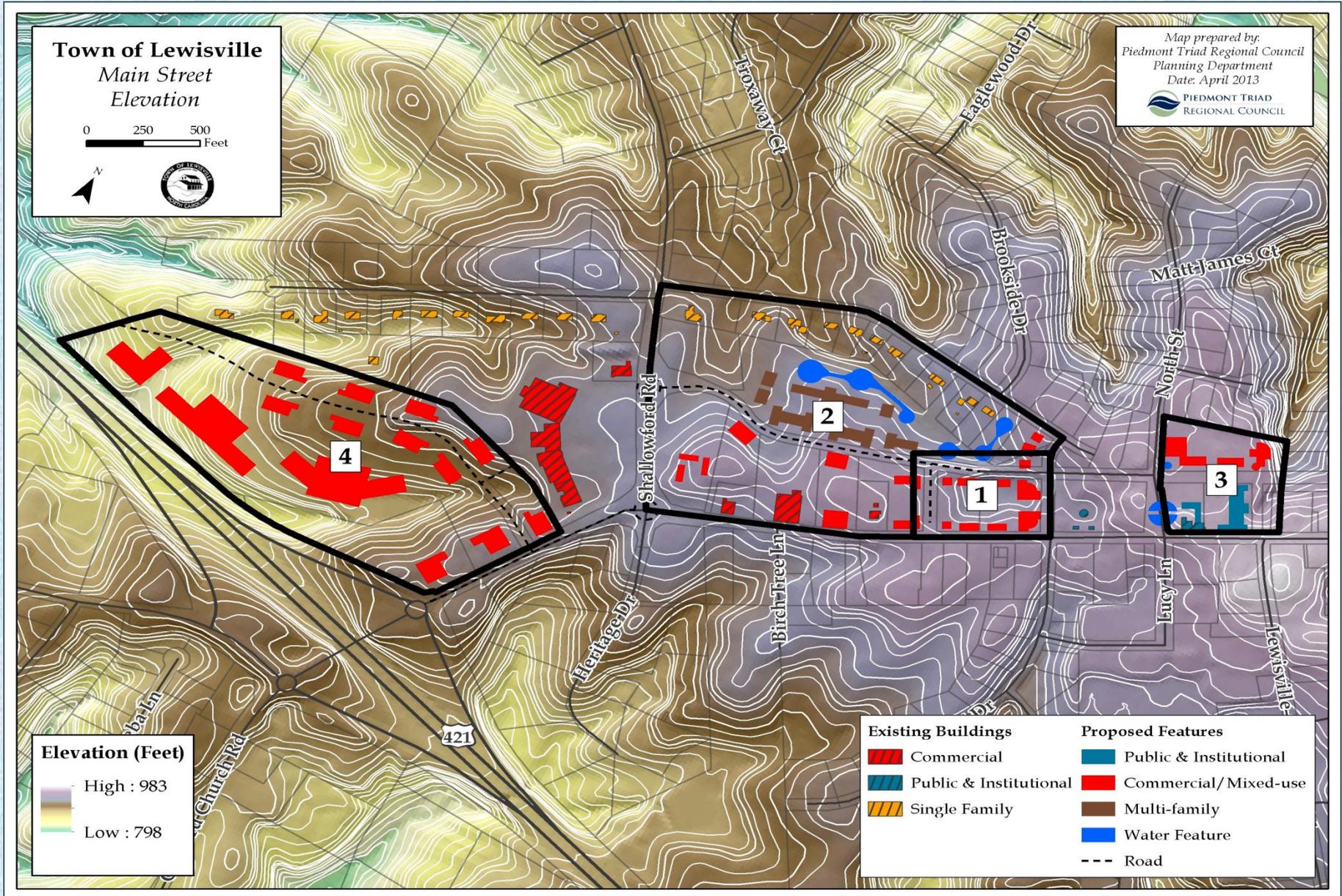
Map prepared by:
Piedmont Triad Regional Council
Planning Department
Date: April 2013
PIEDMONT TRIAD
REGIONAL COUNCIL

Proposed Features

-  Commercial/Mixed-use Building
-  Parking
-  Road
-  Sidewalk



Composite Plan – Focus Areas 1 Thru 4





GREAT WAGON ROAD STREETSCAPE AND DESIGN IDEAS

Great Wagon Road – History

In 1996, the Town of Lewisville commissioned the DSAtlantic Corporation to create a conceptual design for a one-way westbound roadway that would run parallel the existing Shallowford Road. This design would allow for the conversion of Shallowford Road to a one-way, eastbound roadway.

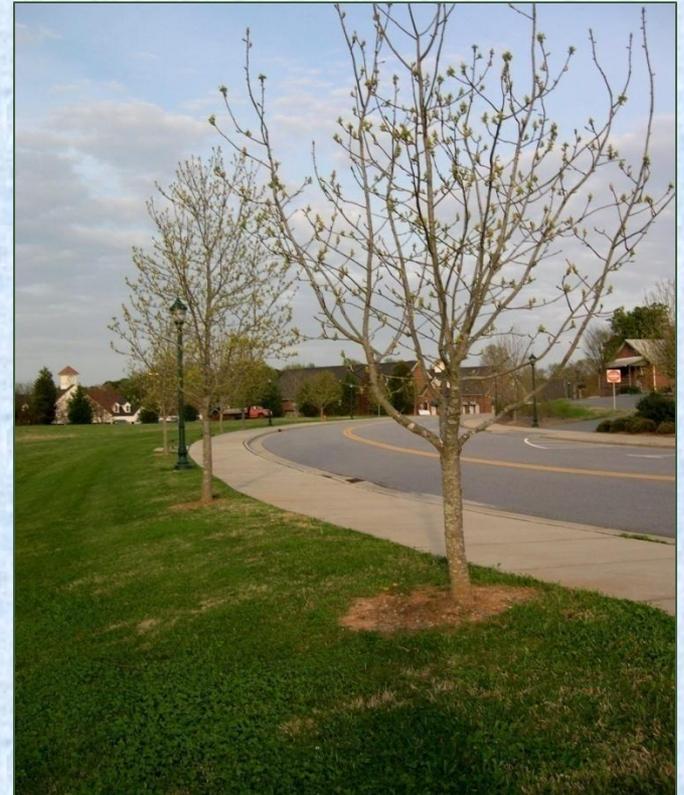
Town officials hoped that the creation of one-way parallel roadways would preserve the character of the Town's downtown area, promote further development, and safely and efficiently move traffic in one of the fastest growing communities in Forsyth County.

In 2011, right-of-way acquisition was completed for the roadway. Monies have been requested by the Town from the Winston-Salem MPO to permit design work to begin.



Great Wagon Road Streetscape – Existing

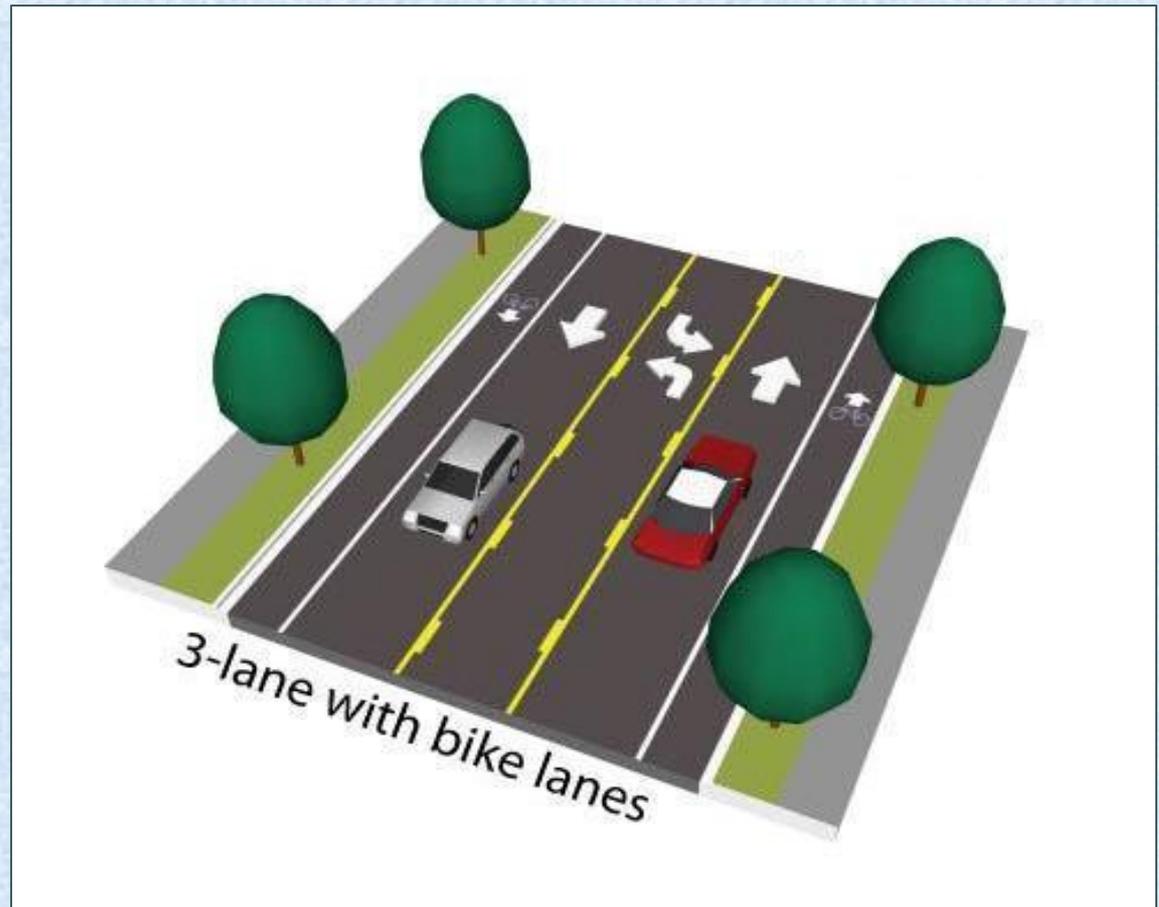
Roadway located behind Roller Mill



Great Wagon Road Streetscape Options

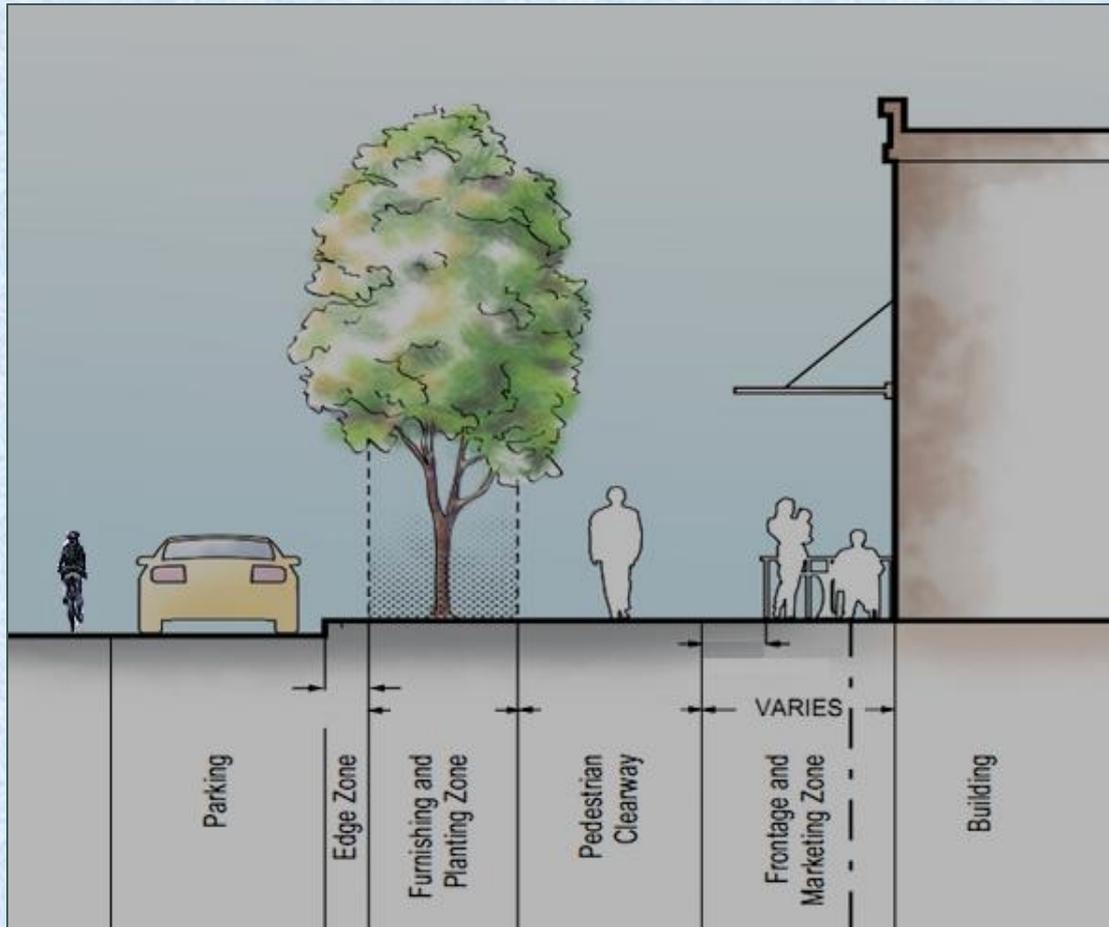
Include 'Complete Streets' Elements in the Design of the New Roadway

“Complete Streets are streets for everyone. They are designed and operated to enable safe access for all users, including pedestrians, bicyclists, motorists and transit riders of all ages and abilities. Complete Streets make it easy to cross the street, walk to shops, and bicycle to work.”



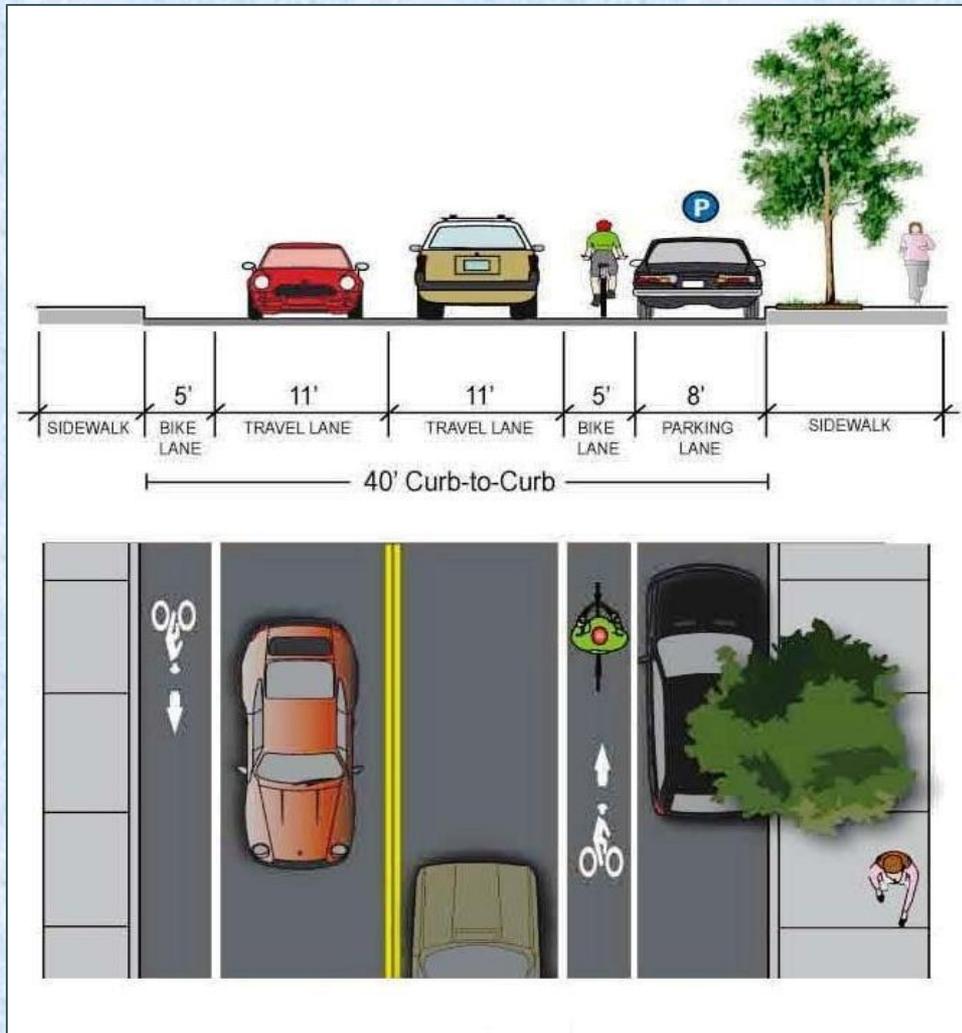
Great Wagon Road Streetscape Designs

Possible Designs for New Roadway



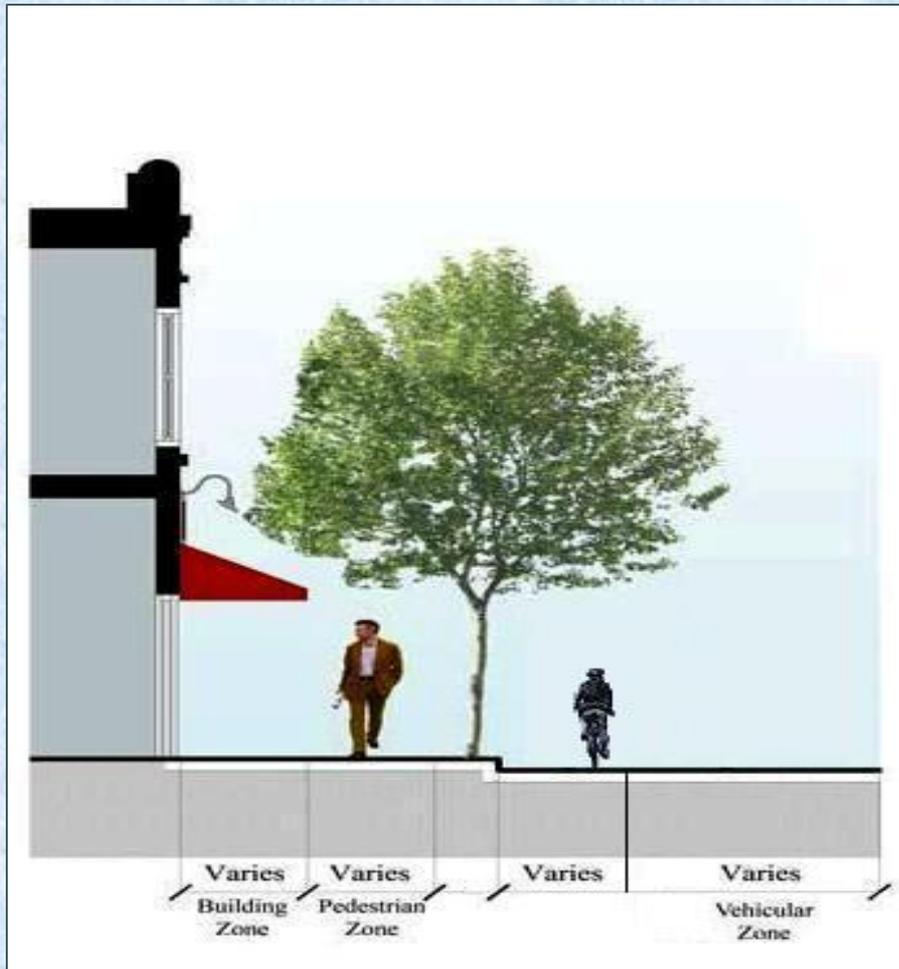
Great Wagon Road Streetscape Design Alternatives

Possible Designs for New Roadway



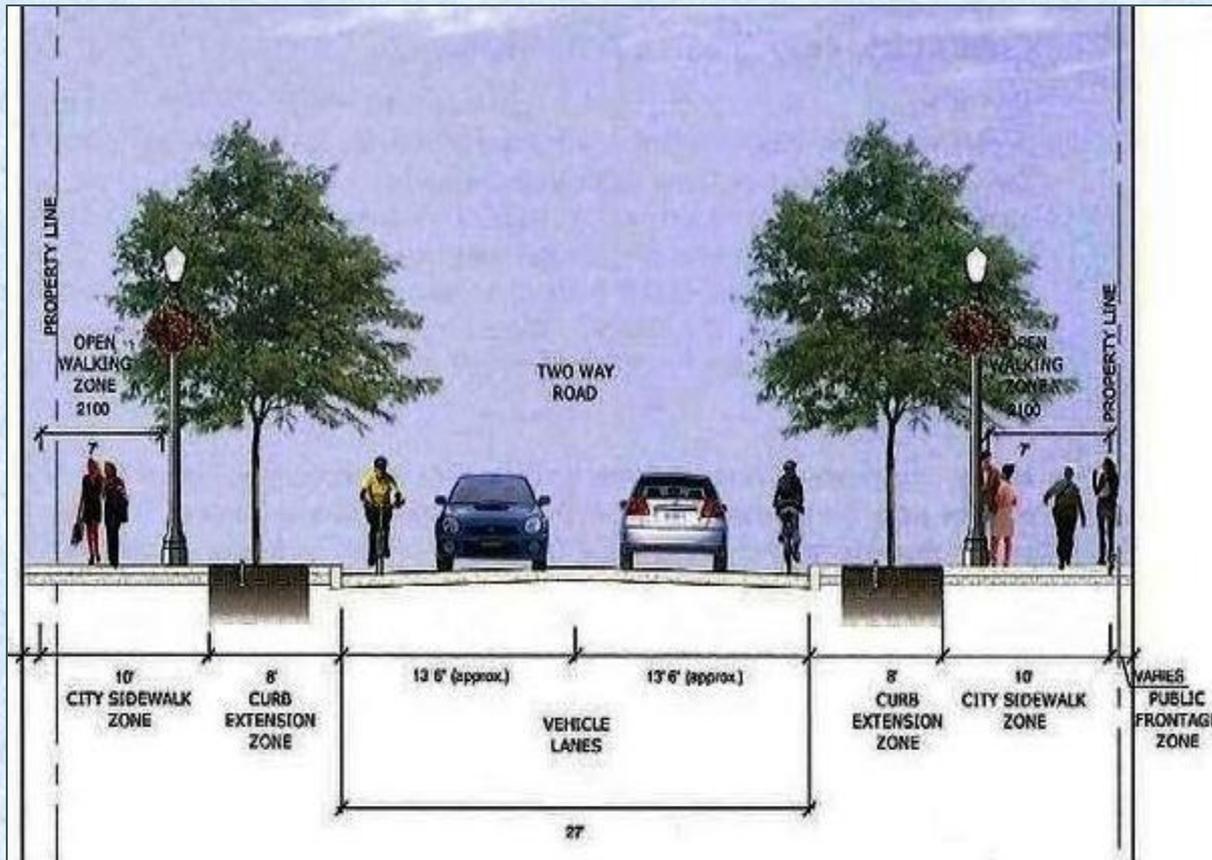
Great Wagon Road Streetscape Design Alternatives

Possible Designs for New Roadway



Great Wagon Road Streetscape Design Alternatives

Possible Designs for New Roadway



Kit of Tools

“Elements that are common to the roadway that bring added identity to it that are both functional and aesthetically pleasing to the user.”



Common Architectural Elements

Shallowford Road

Unifying Elements – Reoccurring Use of Materials

“Rock and Stone accents create a village like atmosphere that has a residential feel that promotes a pedestrian scale within the downtown.”



Common Architectural Elements

Shallowford Road

Unifying Elements – Rock / Stone Accents



Common Architectural Elements

Shallowford Road

Unifying Elements – Rock / Stone Accents



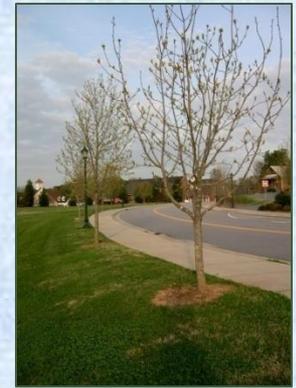
Common Architectural Elements

Shallowford Road

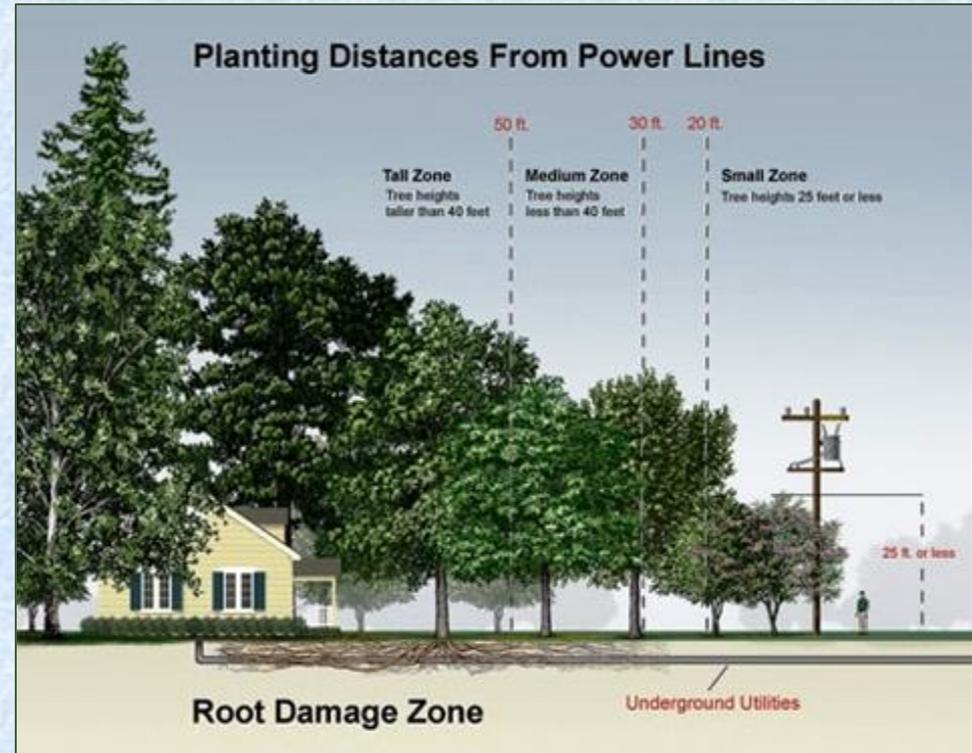
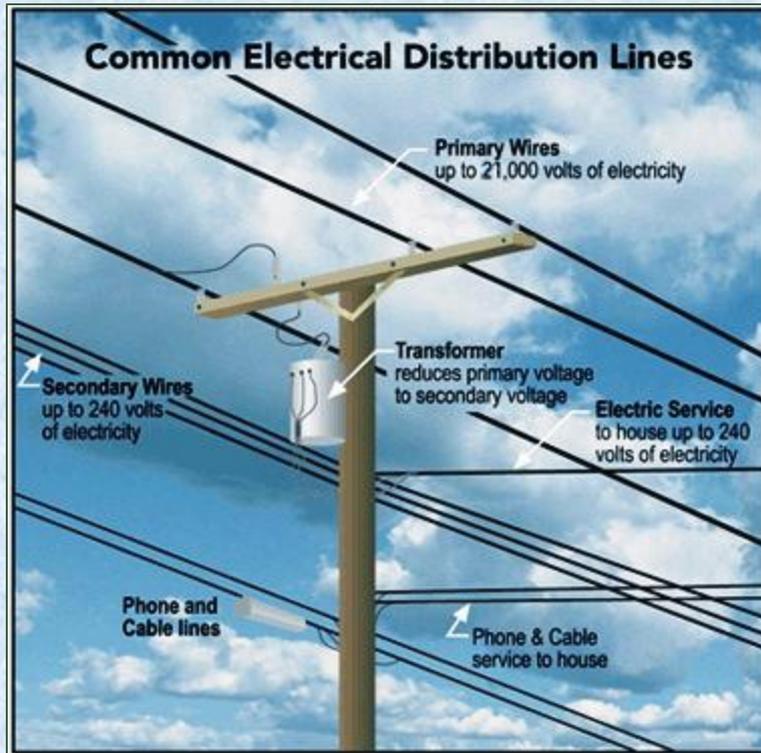
Unifying Elements – Rock Stone Accents



Great Wagon Road Streetscape Design Alternatives



Unifying Elements – Underground Power Lines



Great Wagon Road Streetscape Design Alternatives



Unifying Elements – Pedestrian Walkways and Sidewalks



Great Wagon Road Streetscape Alternatives

Unifying Elements – Decorative Benches



Great Wagon Road Streetscape Alternatives

Unifying Elements – Decorative Trash Cans



Great Wagon Road Streetscape Alternatives

Unifying Elements – Decorative Bollards



Great Wagon Road Streetscape Alternatives

Unifying Elements – Decorative Street Lighting



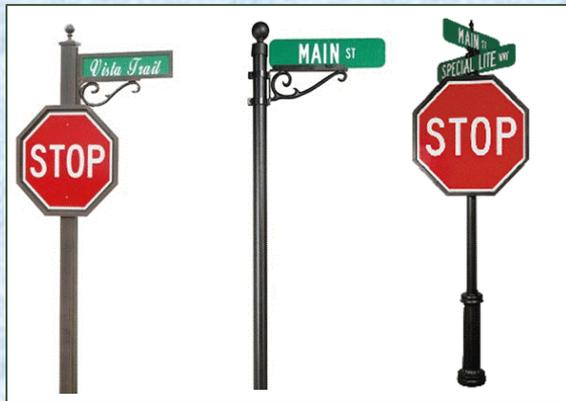
Great Wagon Road Streetscape Designs

Unifying Elements – Regulating Traffic Signs



Great Wagon Road Streetscape Design Alternatives

Unifying Elements – Regulating Traffic Signs



Great Wagon Road Streetscape Design Alternatives

Unifying Elements – Regulating Traffic Signs



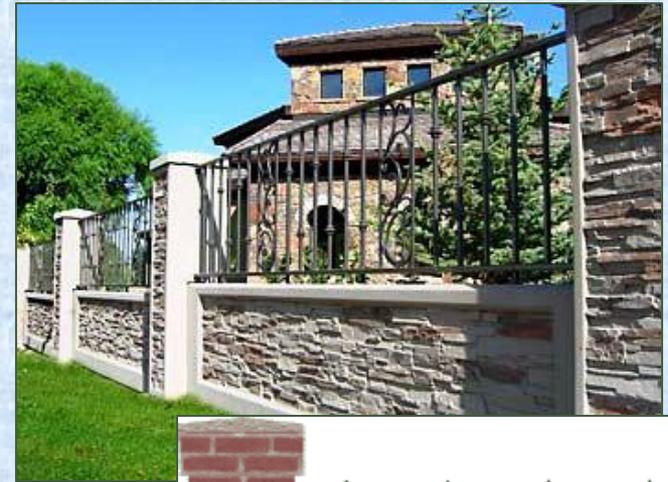
Great Wagon Road Streetscape Design Alternatives

Unifying Elements – Stamped Pedestrian Crosswalks



Great Wagon Road Streetscape Design Alternatives

Unifying Elements – Decorative Walls and Fences



Questions?



Next Steps:

- **Feedback from Town Council members on progress to date.**
- **Finalize merger of the DTO with LD1 and LD2**
- **Weigh and develop streetscape recommendation**
- **Develop additional ordinances for supersites**
- **Revisit specific uses for various categories and sites**
- **Continue review of Town doctrines (Comprehensive Plan, Greenway Plan and Legacy) to assess congruence with development of this plan**
- **Continue exploration of “best practices” in other like towns to glean ideas and possibilities and lessons learned**

